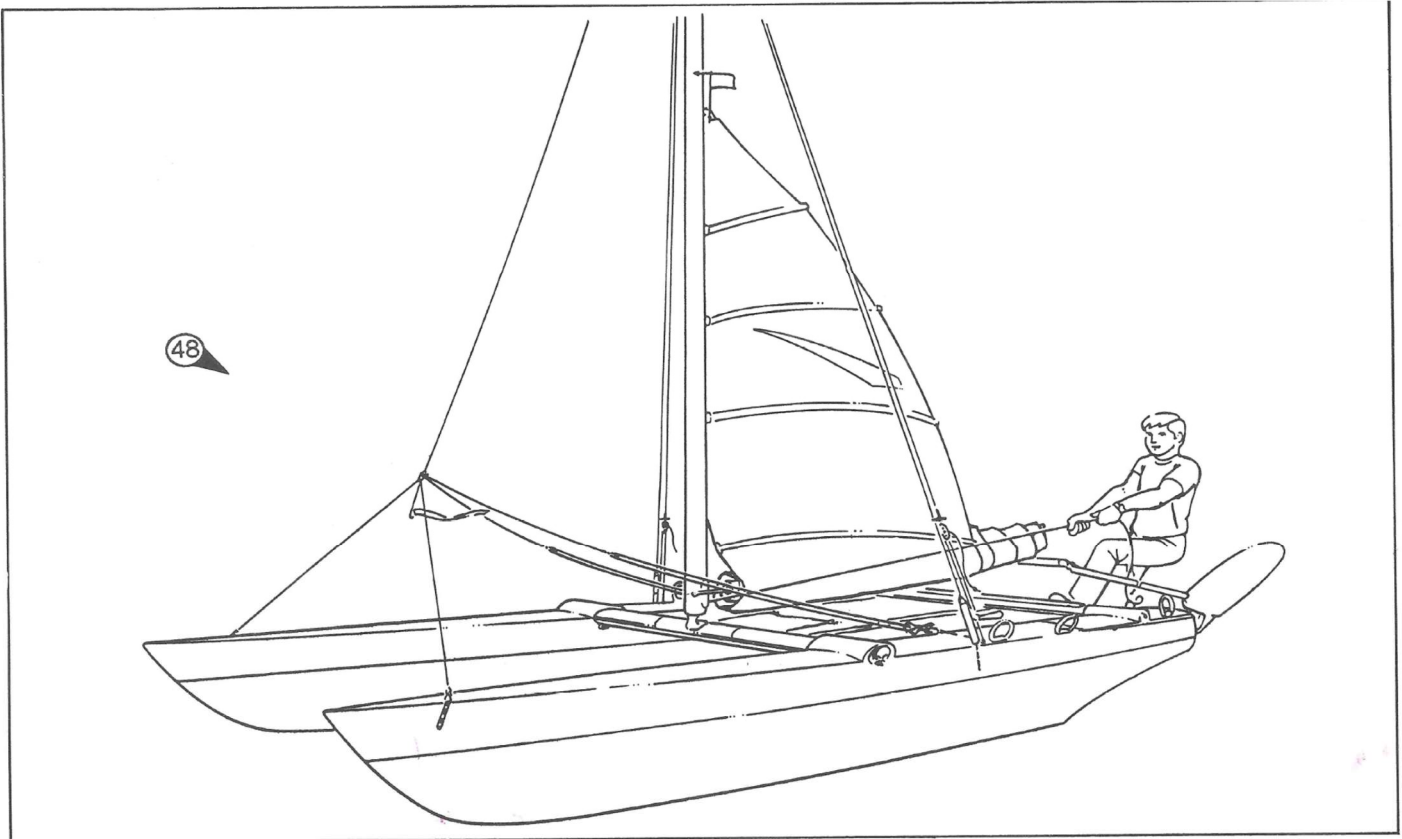


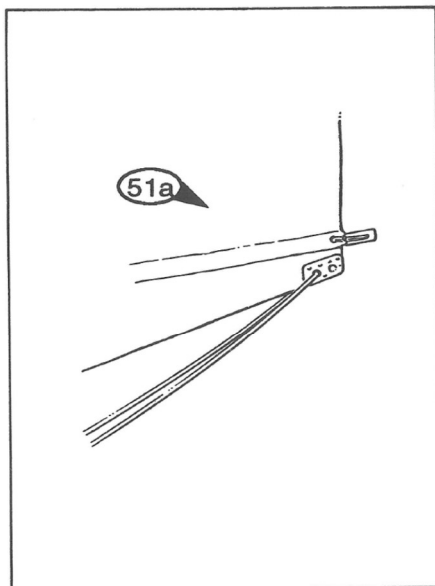
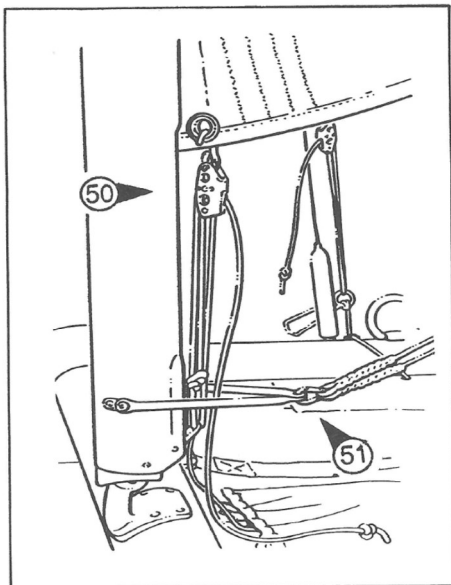
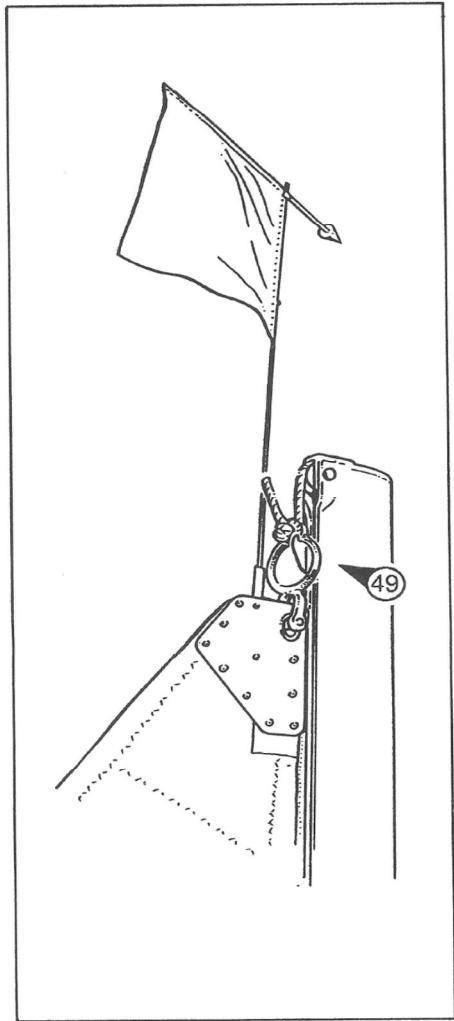
*Dart 18*

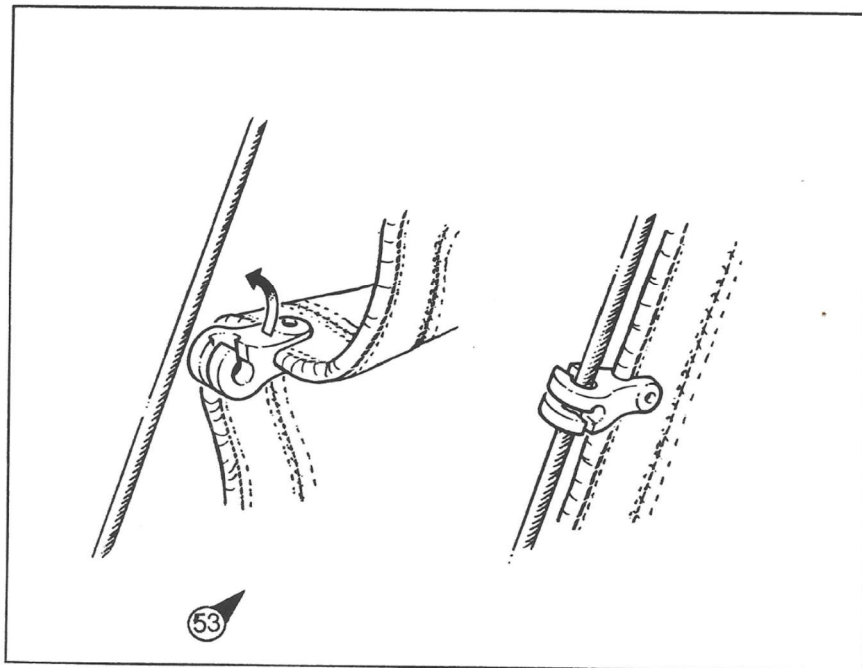
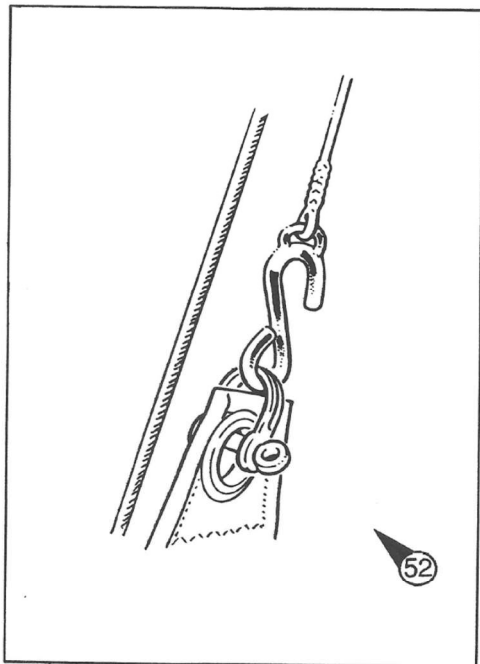


***DART 18***  
***RIGGING MANUAL***



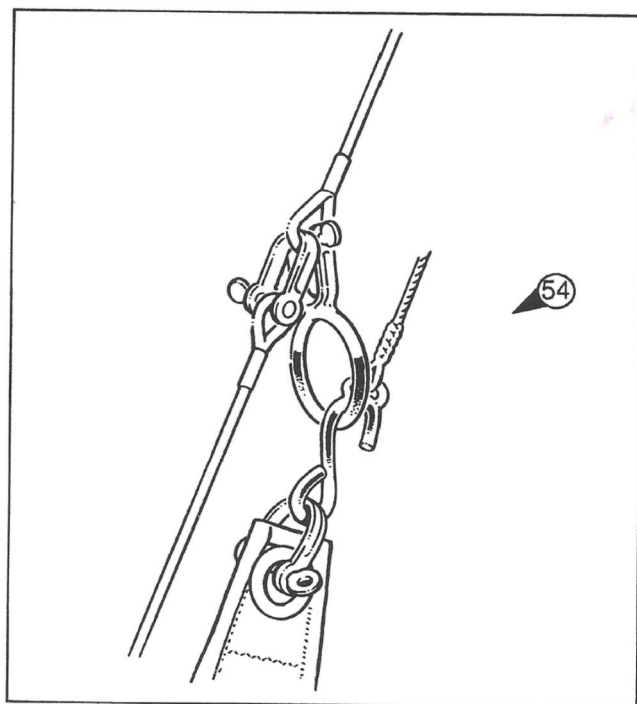
- \* Check that the halyard is running up the line of the mast groove, NOT AROUND THE MAST HOOK, and raise the sail by pulling the halyard where it exits from the base of the mast (Fig 48) ▽. The mainsail is secured at the top of the mast by the halyard lock and the last 15cm of mainsail hoist should be achieved with a firm pull to ensure the locking ring is positioned over the hook as shown (Fig 49) ▲
- \* Feed the tack of the sail down into the lower part of the luff groove and connect the mainsail down haul to the eye as shown (Fig 50) ▽. At this point no tension should be applied to the sail luff. The main sheet assembly should only be connected to the clew eye when the boat is in the water and you are about to go sailing.
- \* Identify the spanner line, Part AF, feed it through the clew eye and clip to the mast spanner as shown (Fig 51 & 51A) ▽. Store spare halyard in lower pocket of the trampoline bag.



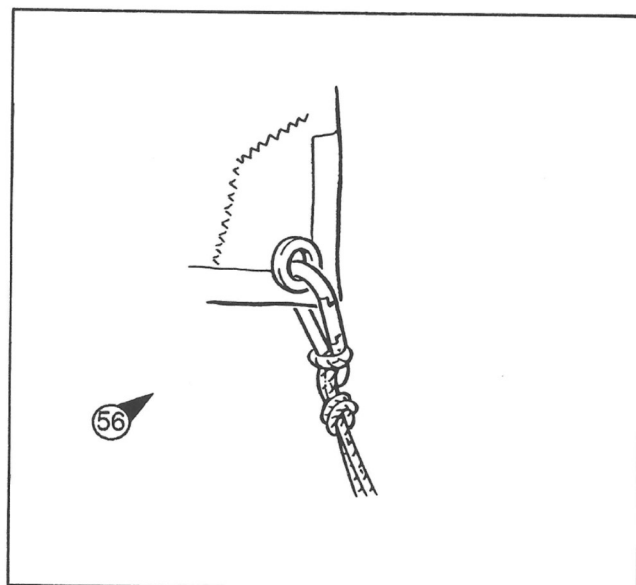
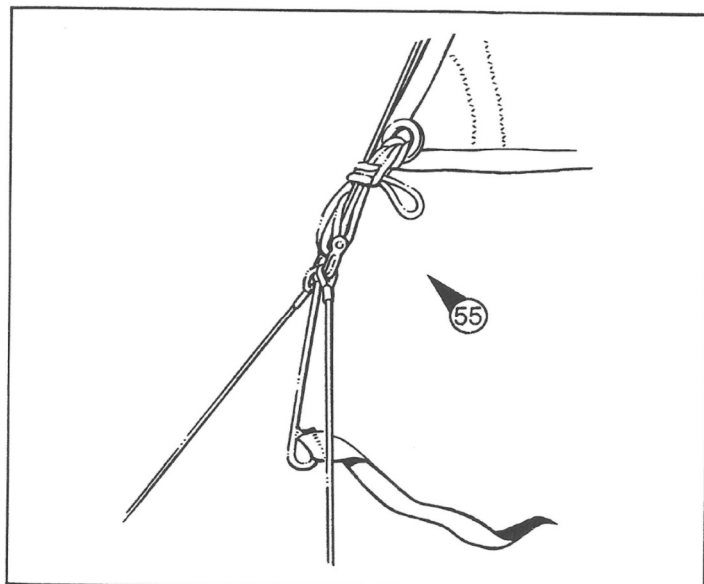


### RAISING THE JIB

- \* Unfurl the sail and connect the head to the halyard as shown (Fig 52)▲. It is important that the 'S' hook is positioned with its open edge facing aft.
- \* Hoist the sail from where the halyard exits at the base of the mast clipping each sail hank to the fore-stay using a push and turn action (Fig 53)▼.
- \* Hoist the sail through the lock ring and lower gently by tensioning the jib clew causing the hook to lock (Fig 54)▶.
- \* Coil the excess halyard and secure it in the under pocket of the trampoline bag.



- \* Tension the luff of the jib using the down haul line as shown (Fig 55)▼. Care should be taken to ensure that the tension of the sail luff is no more than the tension of the fore-stay.
- \* Connect the jib sheet clip to the sail clew as shown (Fig 56)▲.
- \* Identify the paddle Part AL, and store it in the trampoline bag ready for use.



## YOUR FIRST SAIL

Your boat is now ready to sail but before you launch, it is a good idea to familiarise yourself with the boat's equipment, personal safety, and the technique of lifting and lowering the rudders.

## PERSONAL SAFETY

The DART 18 is an excellent compromise between FUN, SPEED AND SAFETY, we recommend you always wear suitable clothing and adequate buoyancy aid/life jacket when venturing afloat.

## RIGHTING IN THE EVENT OF CAPSIZE

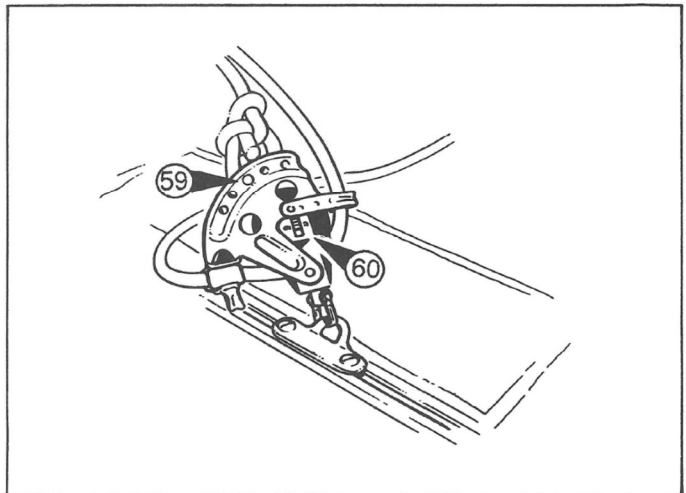
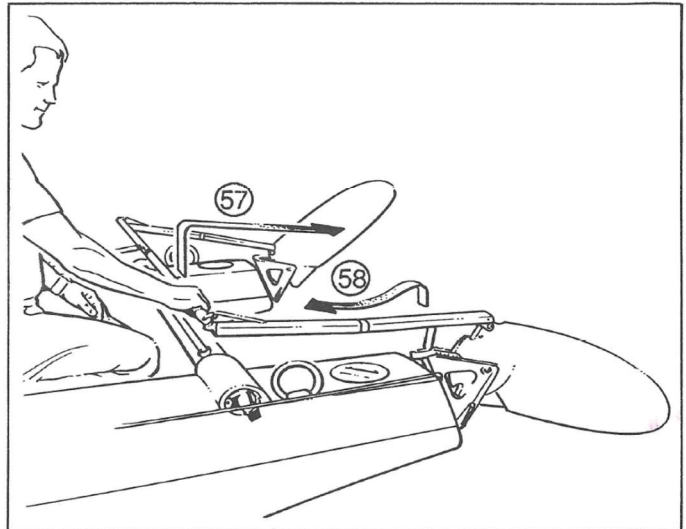
The DART 18 can be righted from a total inversion or a 90° knock down. Good use should be made of the main halyard or jib sheets as convenient righting lines. NB. Catamarans can only be righted when the mast is pointing towards the wind. Further details of the correct righting procedure of catamarans is available in the Catamaran Book.

## LIFTING AND LOWERING RUDDERS

The rudders may be lowered independently.

To Lower a rudder lift the tiller arm via the connecting bar and gently push backwards (Fig 57) until the lock roller drops into the lower slot of the rudder blade.

To lift the rudders, hold the connecting bar close to the tiller end, lift and pull in a scoop action (Fig 58) allowing the roller to follow the rudder cam into the upper rudder slot. NB. It is a good idea to lift and lower rudders independently to ensure some steerage control whilst sailing on and off the beach.



## JIB AND MAIN SHEET BLOCKS

All ratchet blocks can be adjusted to allow quick release of sheets from the jamming cleats (Fig 59) there is also an ON/OFF ratchet facility for sailing in strong winds (Fig 60) ▶

## LOWERING THE MAIN SAIL

To lower the main sail it is necessary to detach the halyard ring from the mast head lock. This sequence is simple but needs to be accurately followed.

1. Place the boat **head to wind**.
2. Disconnect the spanner control line.
3. Disconnect the main sail down haul and remove the lower sail luff from the mast groove.
4. Pull the halyard firmly to lift the halyard ring off the hook, **retain the tension**.
5. Turn the mast to port (left).
6. Release the halyard and pull the tack of the main sail downwards, make sure the mast is still rotated to port.
7. Once the sail has been pulled down past the hook, the mast can be released and the sail lowered in a normal fashion.
8. It is a good idea to roll the sail as it is lowered to prevent it from being blown away.

## LOWERING THE JIB

1. Release the jib down haul line.
  2. Raise the jib via its halyard.
  3. Lower the jib by pulling down on the sail tack.
- NB. This action should be carried out in a quick, smooth action, ie up/down so that the jib hook falls through the lock ring.